



Marshalls

Landscape Protection

**Concrete Cycle
Segregation Unit**

Cycling landscapes

Cycling offers a number of social, economic and health benefits. While it is broadly acknowledged that the Netherlands and Denmark are the leading cycling nations, other countries throughout Europe, United Kingdom and further afield are also moving forward.

With an increasing number of people commuting by foot and bike, we believe that in order to make cities safer and more accessible, the decision-makers must realise that planning for people is key.

When deciding where to place bicycle infrastructure, connectivity and safety is of the utmost importance. We recognise the need for well-designed cycling solutions to achieve functional, attractive and fully integrated cycle schemes for the transport future of our towns and cities.

At Marshalls Landscape Protection, we offer a number of Active Travel Solutions to enable the public to cycle and walk safely, whilst encouraging these methods of commuting.



Why choose cycling?

Cycling plays an important role in protecting our environment and improving general health and wellbeing, providing the public with physical activity and thereby saving funds from public health budgets.

Encouraging more people to cycle is being seen as a vital part of any local authority plan to:

- Tackle congestion on public transport systems
- Improve air quality in urban areas and reduce carbon footprint
- Promote physical activity, health and wellbeing
- Improve accessibility for all, minimising time, distance and effort

The Marshalls **Concrete Cycle Segregation Unit**

Manufactured in Britain, our versatile concrete kerb unit is designed to act as a demarcation tool to safely segregate cyclists from trafficked areas.

Its 45 degree splayed kerb profile on the cycleway side provides a soft transition between the carriageway and the cycle lane. This means that cyclists of all abilities can use the cycleway without the fear of colliding with a steep kerb or catching a pedal.

The outside kerb offers a bullnose profile designed to stop vehicles mounting the unit and potentially endangering the cyclist.

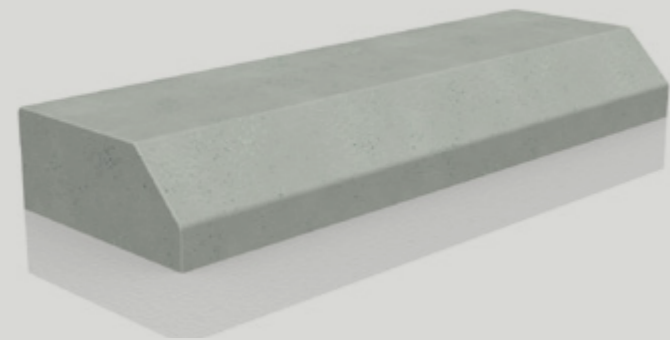
Designed to be installed in new or existing schemes where the highway requires a redesign, the Cycle Segregation Unit ensures that cyclists feel safe and at ease when commuting, and consequently encouraging an increase in this mode of transport.



Features & benefits

- 01 Creates a physical demarcation for cyclists allowing them to travel safely and easily
- 02 Provides a good level of containment of vehicles
- 03 Offers quick and simple installation
- 04 Allows a good platform for additional signage and further demarcation
- 03 Encourages cycling, leading to benefits in health and well-being and a reduction in carbon footprint
- 04 Made in Britain
- 05 Complies with the performance levels in the European Standard - BS EN: 1340
- 06 Manufactured with concrete, therefore reducing maintenance costs and lowering carbon footprint over its life cycle

Materials, colours and finishes



Materials

The Marshalls Cycle Segregation unit is manufactured using concrete and finished to give a smooth surface.

Resistant to water and high temperatures, concrete is known for its resilience and strength and brings multiple benefits when used for commercial projects.

One of the main advantages of using this material is that it requires little or no maintenance, therefore reducing long-term repair costs. This also brings a number of sustainability benefits over its life cycle.

Colours



Smooth Grey

Finishes

Smooth surface texture

Product specifications



Left-hand and right-hand unit

Overall height (mm)	255
Height above ground (mm)	125
Length (mm)	1000
Width (mm)	300
Weight (kg)	171
Radius end (mm)	150
Fixing method	Root fixed
Root depth (mm)	130
Carbon footprint (CO ₂ /kg)	20

Please note that carbon footprint figures are estimates based on full 'cradle to grave' footprints for similar products, calculated by the Carbon Trust.

Intermediate unit

Overall height (mm)	255
Height above ground (mm)	125
Length (mm)	1000
Width (mm)	300
Weight (kg)	179
Radius end (mm)	N/A
Fixing method	Root fixed
Root depth (mm)	130
Carbon footprint (CO ₂ /kg)	21

Please note that carbon footprint figures are estimates based on full 'cradle to grave' footprints for similar products, calculated by the Carbon Trust.

Installation guidance

With an overall height of 255mm, the Cycle Segregation Unit is root fixed 130mm below the finished ground level.

The units may be bedded directly onto a freshly mixed ST4 strength concrete kerb race, or bedded onto mortar on a hardened kerb race foundation with a minimum thickness of 150mm. Mesh reinforcement can be used within the race if required.

When laying, units should be bedded onto the bedding layer using a paviours maul to line and level. String lines should be used to ensure the accuracy of the units being installed to line and level.

Backing should extend to a level of 50mm below the surface on either side of the unit. Both the backing and foundation race should form one monolithic structure.

Units should be close jointed, leaving dry gaps of 2mm minimum. Where units are laid over a jointed concrete pavement, suitable joints should extend through the line of units at a joint and continue through the haunching concrete.

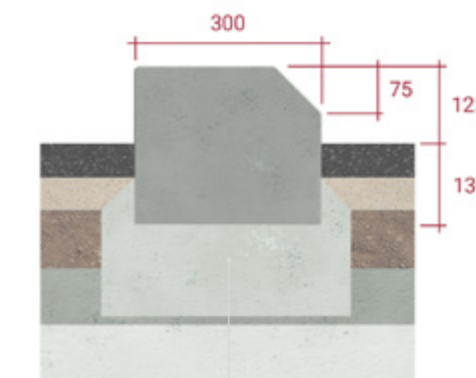
For technical advice on commercial installations, or when confronted by unusual problems or circumstances, please contact Marshalls Technical Advisory Services:
Telephone: **0370 411 2233**
Email: **advisory.services@marshalls.co.uk**

*Please refer to the current British Standards and Codes of practice when installing and the full Marshalls Installation document.



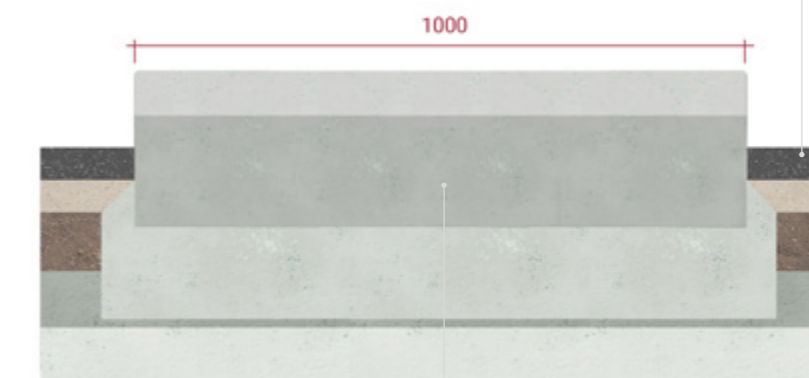
Installation illustrations

Side profile



150mm ST4
Concrete Race

Front profile



255mm deep unit laid into race of ST4 within
carriageway or if race allowed to harden
bedded on a 25-50mm thick Type B Mortar

Please note:

- Type B Mortar 25-50mm to be inline with BS7533
- Haunching must extend to 50mm below surface
- Haunch and race must be monolithic

All units are pre-formed in factory-controlled conditions, ensuring design accuracy and specification validity. Kerbs comply with BS EN:1340.

Our approach to sustainability

We help to build safe, sustainable and beautiful spaces for everyone.

Our approach to sustainability is underpinned by our belief in doing the hard work in areas that are material to our business and our stakeholders, by working with leading organisations and by getting external validation.



ISO50001, which we achieved and maintained since 2015, enables organisations to establish the systems and processes necessary to continually improve energy performance

SBTi targets to **reduce Scope 1 and 2 greenhouse gas emissions** 40% per tonne of production by 2030



100% of our employees are paid at, or above, **the Living Wage Foundation level**



All our production plants now use **100% renewable energy**

We achieved, and have since maintained, **ISO14001 Environmental Management** since 2000. The intentions of **ISO14001** is to achieve a balance between the environment, society and the economy is considered essential to meet the needs of the present without compromising the ability of future generations.



This international standard provides organisations with a framework to **protect the environment** and respond to changing environmental conditions in balance with socio-economic needs



We launched the highly regarded **Power of Logistics** programme to tackle Modern Slavery and led the sector in tackling the issue in the UK

We have eradicated child labour from Tier 1 of our Indian Sandstone supply chain – this is really important to us and our commitment to respecting **human rights** is central to our business



All our Marshalls branded Landscape Protection and Street Furniture products are **made in mainland Britain**



We are proud to have been **FSC®-certified** since 2017. All newly developed Marshalls and RhinoGuard® street furniture products that include wood elements, from now on will be **produced using FSC®-certified (FSC-C133609) hardwood and softwood timber** from well managed forests and other controlled sources. Further detailed reviews are currently underway that will seek to replace any lumber with **FSC®-certified wood** wherever possible



The concrete and natural stone we use to make our products are **recyclable**

We're proud to pay our fair share of tax – we've kept the **Fair Tax Mark** since 2015





 **Marshalls**
Landscape Protection

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